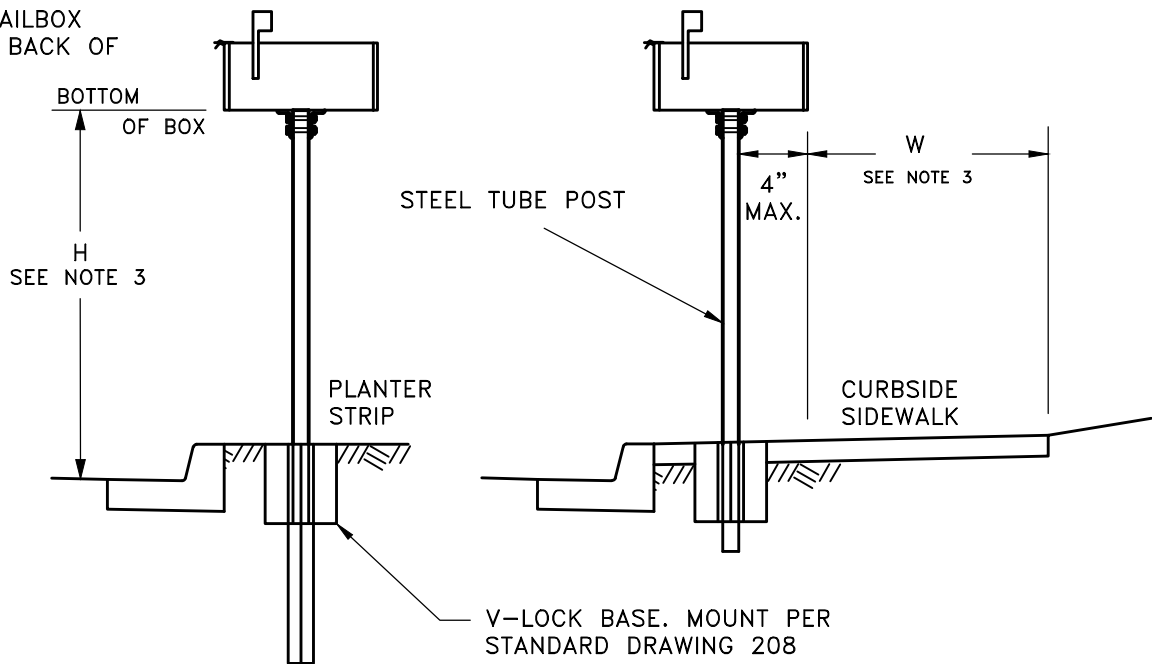


# **APPENDIX C**

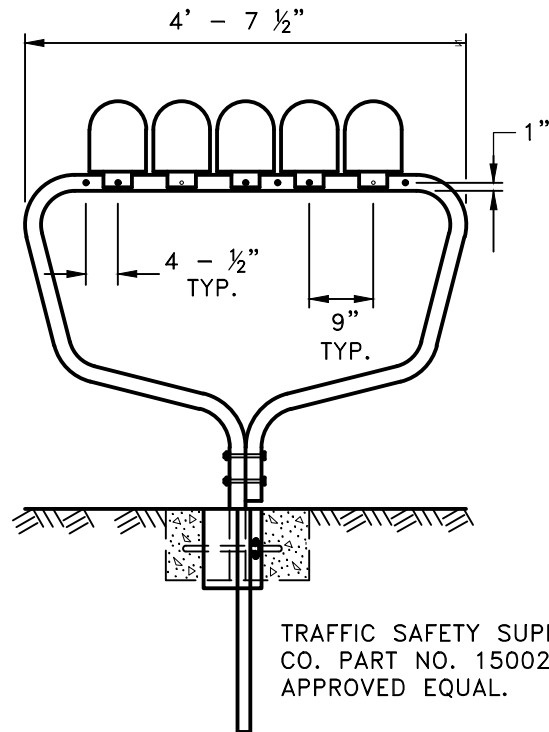
## **UPDATED STANDARD DRAWINGS**

NOTE: The following updated standard drawings shall be used on this project. These drawings supersede and add to standard drawings found in the Standard Construction Specifications. All other standard drawings remain unchanged for this contract.

FACE OF MAILBOX  
EVEN WITH BACK OF  
CURB.



SINGLE SUPPORT



MULTIPLE SUPPORT

GENERAL NOTES

1. CONTACT THE UNITED STATES POSTAL SERVICE FOR LOCATION OF MAILBOX.
2. ON STREETS WITHOUT CURBS, THE FRONT OF THE MAILBOX SHALL BE LOCATED AT THE OUTSIDE EDGE OF THE SHOULDER.
3. W = 6' MIN. CLEAR SPACE FOR LOCAL STREETS.  
7' MIN. CLEAR SPACE FOR ARTERIAL AND COLLECTOR STREETS.  
4.0' MINIMUM WITH CITY ENGINEER APPROVAL
- H = 50" MIN., 52" MAX. ON IMPROVED STREETS.  
36" MIN., 38" ON NON-IMPROVED STREETS.
4. SEE DETAIL DWG. NO. 314 AND DETAIL DWG. NO. 317 FOR REQUIREMENTS RELATIVE TO CONSTRUCTION OF SIDEWALK AROUND OBSTACLES AND MAINTAINING REQUIRED PEDESTRIAN ACCESS CIRCULATION PATH.

TRAFFIC SAFETY SUPPLY  
CO. PART NO. 15002 OR  
APPROVED EQUAL.

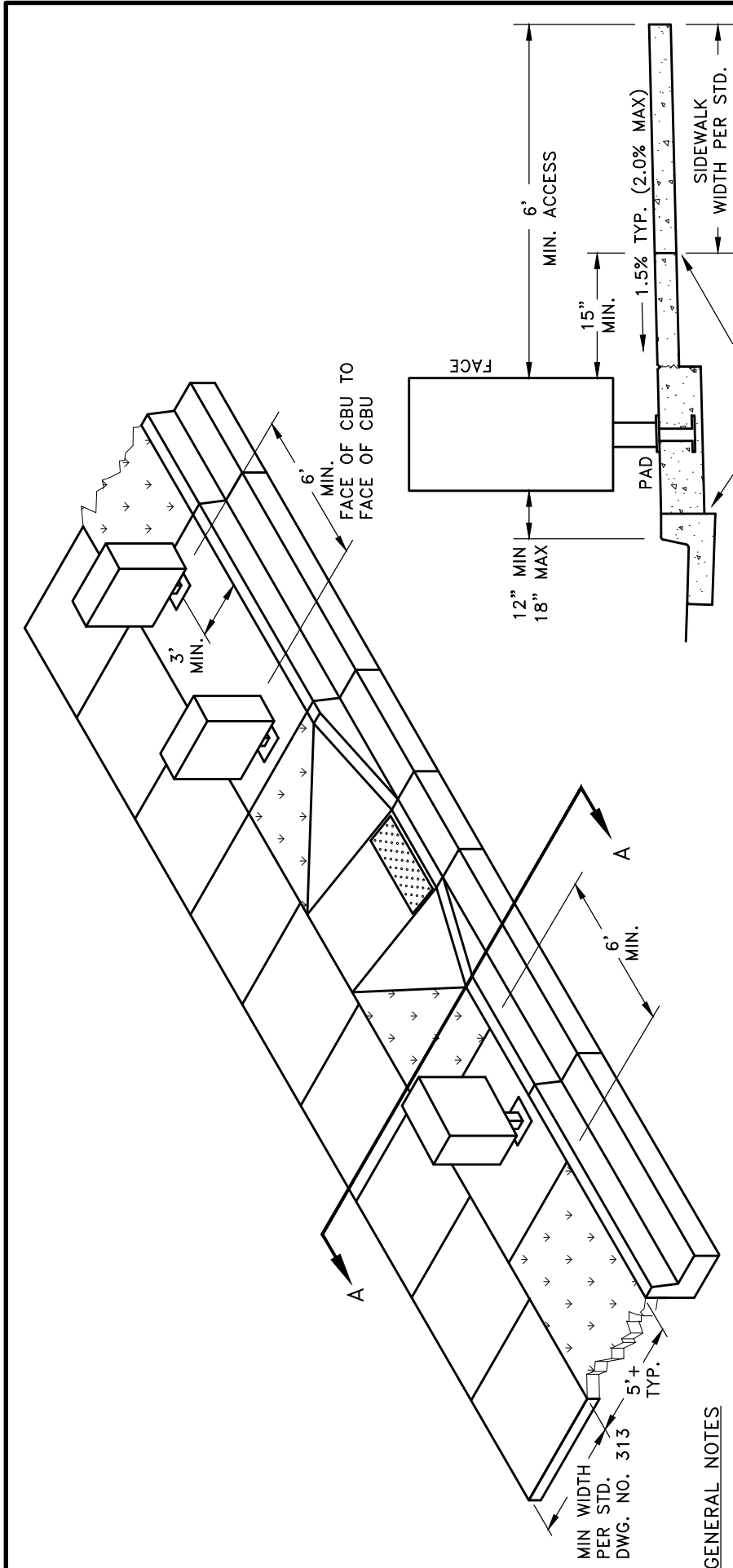
CITY OF ALBANY, OREGON  
PUBLIC WORKS DEPARTMENT

MAILBOX LOCATION

NO SCALE

JANUARY 2023

NO. 202A



**SECTION A-A**

**GENERAL NOTES**

1. CONTACT THE UNITED STATES POSTAL SERVICE FOR LOCATION OF CLUSTER MAILBOX UNITS (CBUs). LOCATION SUBJECT TO APPROVAL BY THE CITY.
2. CBUs SHALL BE PROVIDED WITH AN UNOBSTRUCTED ACCESS PAD MEASURING NOT LESS THAN SIX FEET WIDE BY SIX FEET LONG, CENTERED ON THE FACE OF THE CBU.
3. WHERE LOCATED WITHIN A PLANTER STRIP THE ACCESS PAD SHALL BE CONTINUOUS FROM THE BACK OF CURB TO EDGE OF SIDEWALK, SEPARATED BY COLD JOINTS.
4. THE ACCESS PAD SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE AND SHALL HAVE A SLOPE OF 1.5% IN ANY DIRECTION (2.0% MAX.) AND SHALL BE FLUSH WITH ADJACENT SIDEWALK AND TOP OF CURB.
5. ADJACENT SIDEWALKS MAY BE USED IN MEASURING THE AREA OF THE ACCESS PAD.
6. CBUs SHALL BE LOCATED NO MORE THAN FIFTY FEET FROM A CURB RAMP, AS MEASURED FROM THE CENTER OF THE CURB RAMP TO THE CENTER FACE OF THE FURTHEST CBU.
7. CBUs SHALL BE CONNECTED BY A CONTINUOUS ACCESSIBLE WAY (SIDEWALK) TO THE CURB RAMP.
8. CBUs SHALL BE INSTALLED ON STREETS WITH A MAXIMUM GRADE OF 4% UNLESS APPROVED BY THE CITY ENGINEER.

<b>CITY OF ALBANY, OREGON</b>	
<b>PUBLIC WORKS DEPARTMENT</b>	
<b>CLUSTER MAILBOX UNITS (CBUs)</b>	
<b>NO SCALE</b>	<b>JANUARY 2023</b>
<b>NO. 202B</b>	

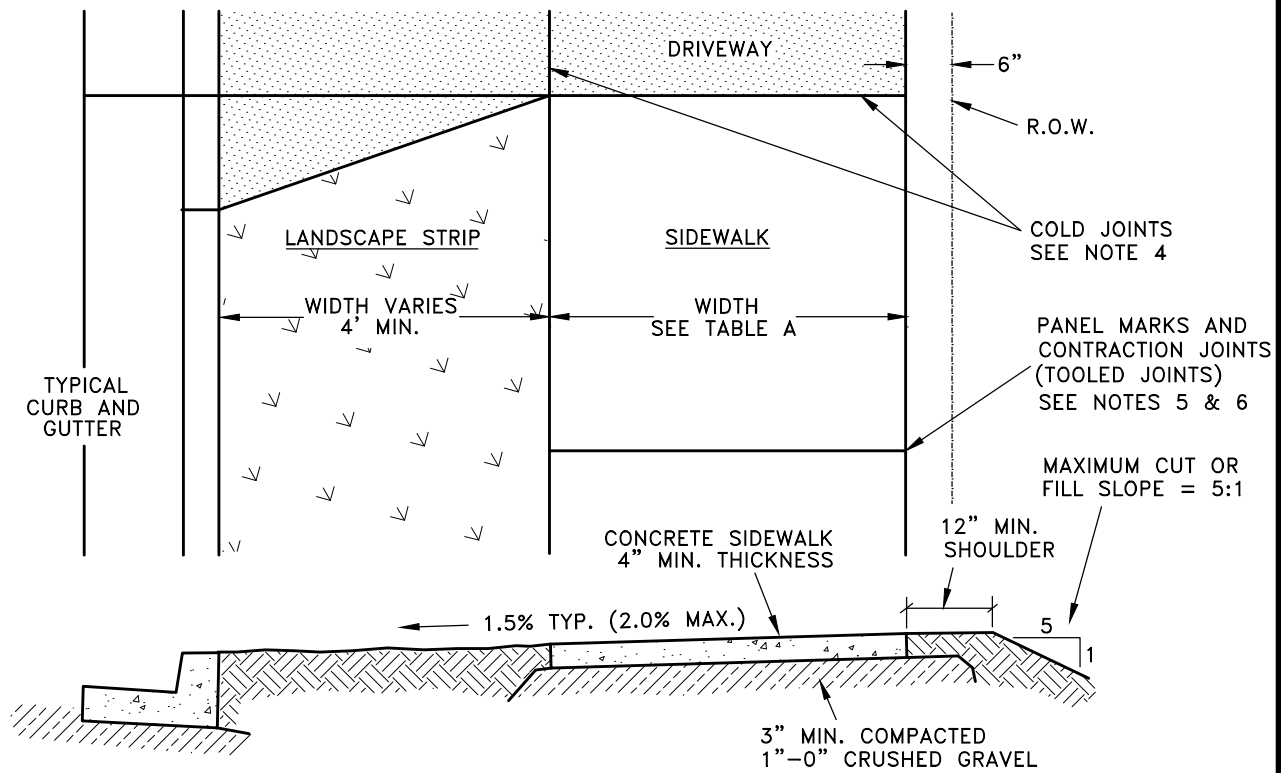
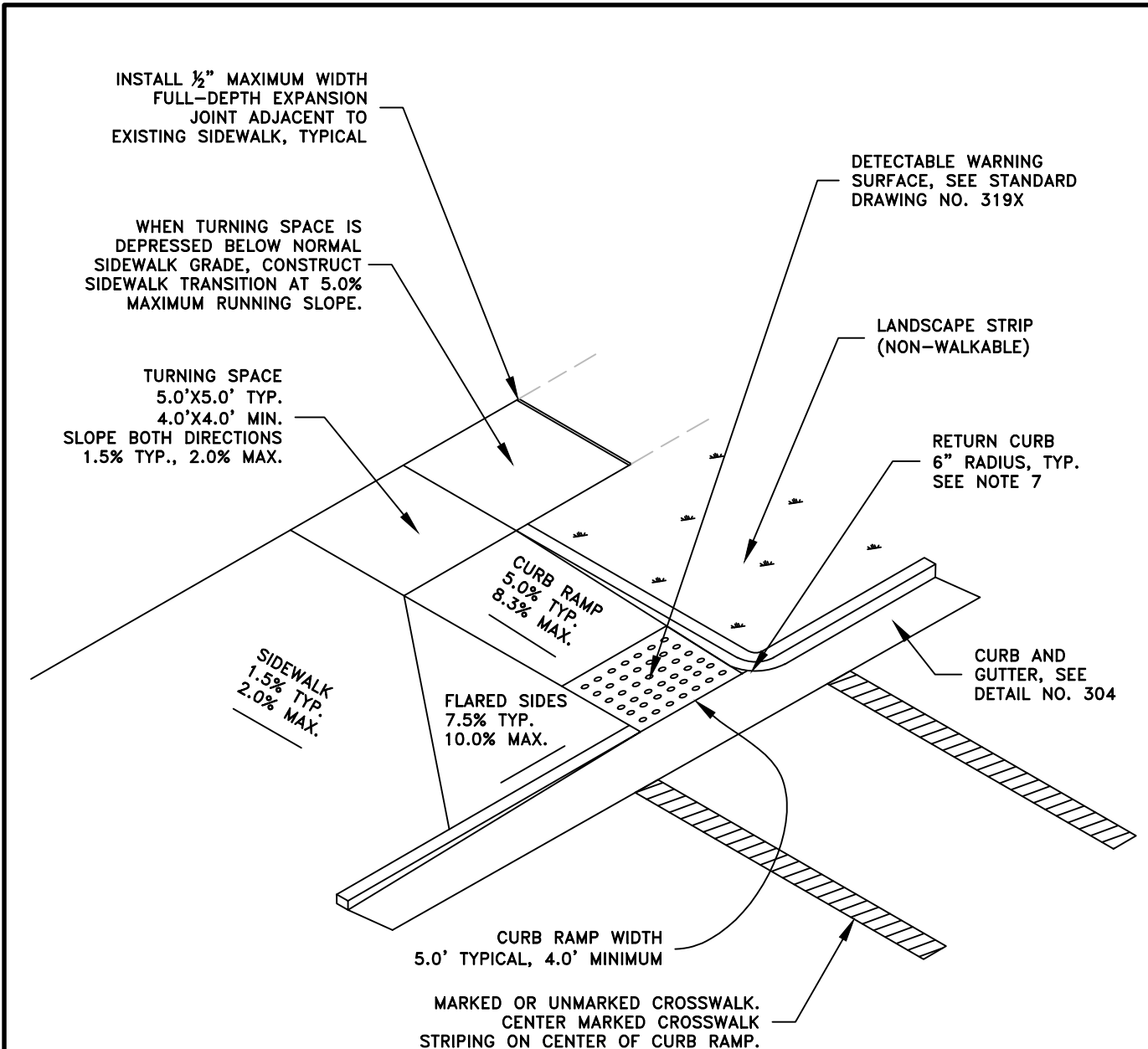


TABLE A STANDARD SIDEWALK WIDTH			CONTRACTION JOINT  $\frac{1}{8}$ " TO $\frac{1}{4}$ " 
STREET TYPE	SETBACK SIDEWALK (STANDARD LOCATION)	CURBSIDE SIDEWALK (APPROVAL REQUIRED)	
ARTERIALS AND COLLECTORS	6 FT.	7 FT.	
LOCAL	5 FT.	6 FT.	

**STANDARD SIDEWALK SPECIFICATIONS**

- STANDARD SIDEWALK SHALL BE CONSTRUCTED A MINIMUM OF 4' BEHIND THE CURB. SIDEWALK CONSTRUCTED ADJACENT TO THE CURB IS NOT PERMITTED WITHOUT APPROVAL OF THE ENGINEER.
- CONCRETE FOR SIDEWALK SHALL HAVE A COMPRESSIVE STRENGTH OF 4000 PSI AND A 2" TO 4" SLUMP.
- THE SIDEWALK AND LANDSCAPE STRIP SHALL SLOPE TOWARD THE CURB AT 1.5% GRADE. HORIZONTAL AND VERTICAL ALIGNMENT SHALL NOT VARY MORE THAN 1/4" FROM ESTABLISHED LINE AND GRADE AS MEASURED WITH A STRAIGHT EDGE.
- COLD JOINTS SHALL BE LOCATED IN SIDEWALKS AT DRIVEWAY SECTIONS AND CHANGES OF DIRECTION. SIDEWALK SHALL NOT BE CONSTRUCTED MONOLITHICALLY WITH CURBING OR OTHER ADJACENT NON-PEDESTRIAN SURFACES. FULL DEPTH JOINT MATERIAL (3 WRAPS MIN. OF NO. 15 ROOFING FELT) SHALL BE PLACED AROUND UTILITY POLES AND FIRE HYDRANTS LOCATED WITHIN THE SIDEWALK AREA.
- THE SIDEWALK SHALL BE DIVIDED INTO EVENLY SPACED PANELS USING A V-GROOVED JOINTING TOOL. PANEL LENGTH SHALL EQUAL SIDEWALK WIDTH.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED AT EVERY OTHER PANEL MARK. DEPTH OF THE JOINTS SHALL BE 1/3 OF CONCRETE THICKNESS.
- INSTALL 1/2" MAXIMUM WIDTH FULL-DEPTH EXPANSION JOINT ADJACENT TO EXISTING SIDEWALK, TYPICAL
- ALL SIDEWALKS SHALL BE FINISHED WITH A "LIGHT BROOM" FINISH. THE DIRECTION OF BROOMING SHALL BE PERPENDICULAR TO THE LENGTH OF THE SIDEWALK. OUTSIDE EDGES OF THE SIDEWALK AND PANEL MARKS SHALL BE FINISHED WITH A V-GROOVED EDGING TOOL.
- SEE DETAIL DWG. NO. 314 FOR SIDEWALK SPECIFICATIONS RELATIVE TO CONSTRUCTION AROUND OBSTACLES (MAILBOXES, POLES, ETC.).
- CONCRETE SIDEWALK ADJACENT TO ROLLED CURB SHALL HAVE A 6" MIN. THICKNESS.

<b>CITY OF ALBANY, OREGON PUBLIC WORKS DEPARTMENT</b>		
<b>STANDARD (SETBACK) SIDEWALK</b>		
NO SCALE	JANUARY 2023	NO. 313



**NOTES:**

1. THE LOCATION AND GEOMETRY OF CURB RAMPS SHALL BE DESIGNED BY THE PROJECT ENGINEER IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT, THE ALBANY DEVELOPMENT CODE, AND THE ALBANY ENGINEERING STANDARDS.
2. ALL CURB RAMPS SHALL COMPLY WITH THE CURRENT ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG) PUBLISHED BY THE U.S. ACCESS BOARD.
3. EACH CURB RAMP SHALL SERVE ONE END OF ONE CROSSWALK. TWO CROSSWALKS SHALL NOT SHARE ONE CURB RAMP.
4. RAMPS SHALL CONFORM TO THE REQUIREMENTS OF STANDARD DRAWING NO. 313 FOR PCC AND BASE.
5. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUN SHALL BE 5% MAX.
6. NO LIP ALLOWED AT BOTTOM OF RAMP
7. RETURN CURB MAY BE PROVIDED IN LIEU OF FLARED SLOPE ONLY IF PROTECTED FROM TRAVERSE TRAVEL BY LANDSCAPING, HANDRAIL OR OTHER OBSTRUCTION. RETURN CURB SHALL NOT REDUCE WIDTH OF APPROACHING SIDEWALK.
8. MAXIMUM REQUIRED CURB RAMP LENGTH SHALL BE 15.0' REGARDLESS OF CURB RAMP SLOPE.
9. THE CITY ENGINEER MAY AUTHORIZE DESIGN EXCEPTIONS FOR ALTERATIONS OF EXISTING FACILITIES WHEN EXISTING PHYSICAL CONSTRAINTS PREVENT FULL COMPLIANCE.

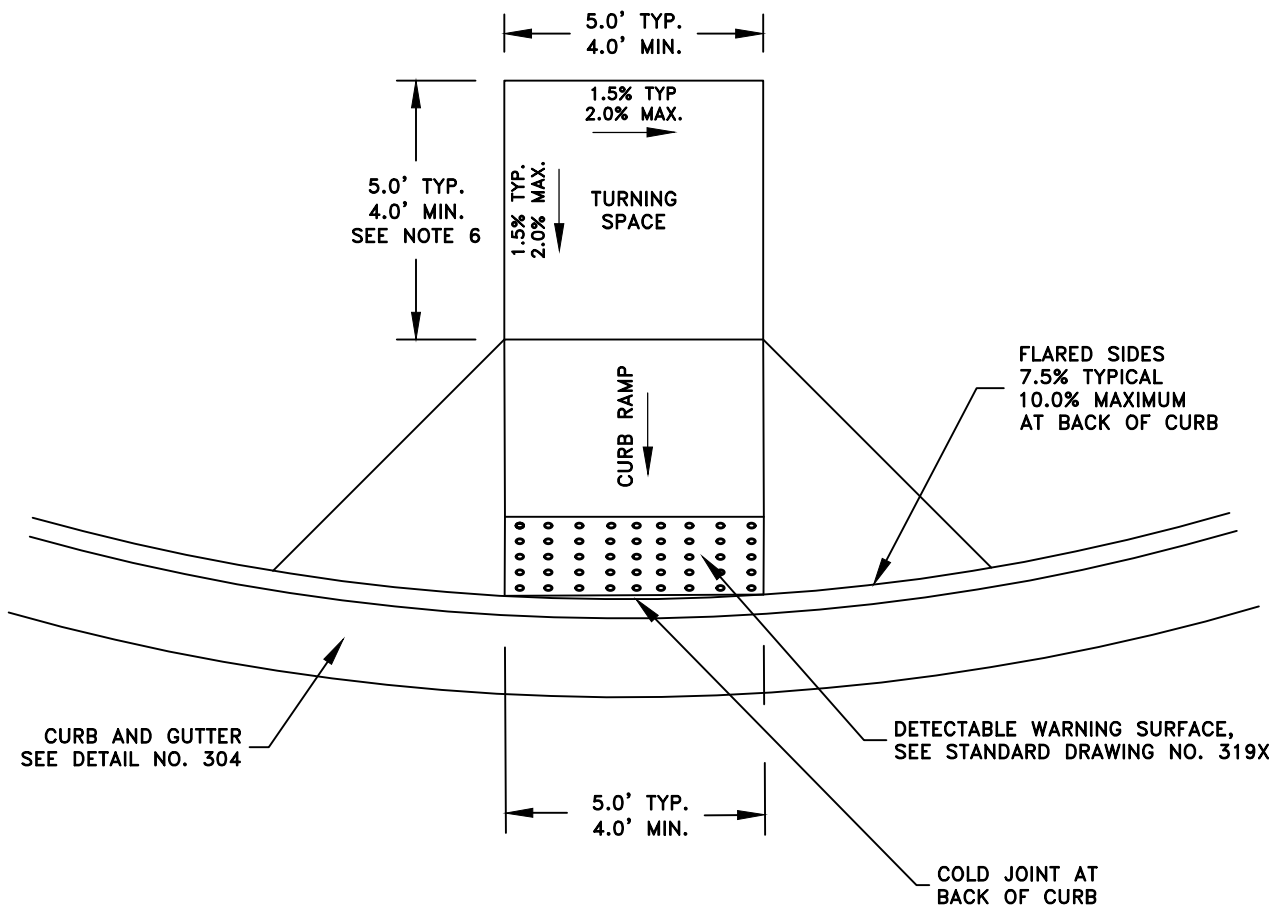
CITY OF ALBANY, OREGON  
PUBLIC WORKS DEPARTMENT

GENERAL CURB RAMP DETAILS

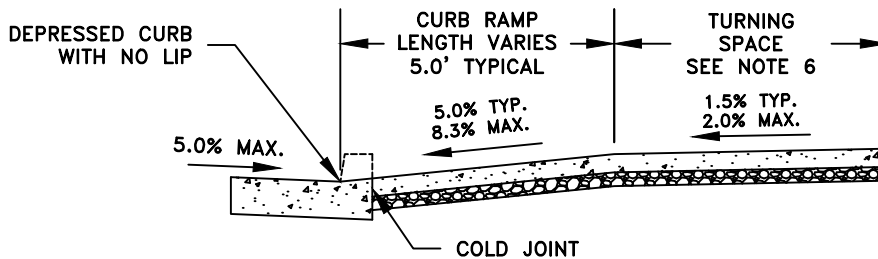
NO SCALE

JANUARY 2023

NO. 315



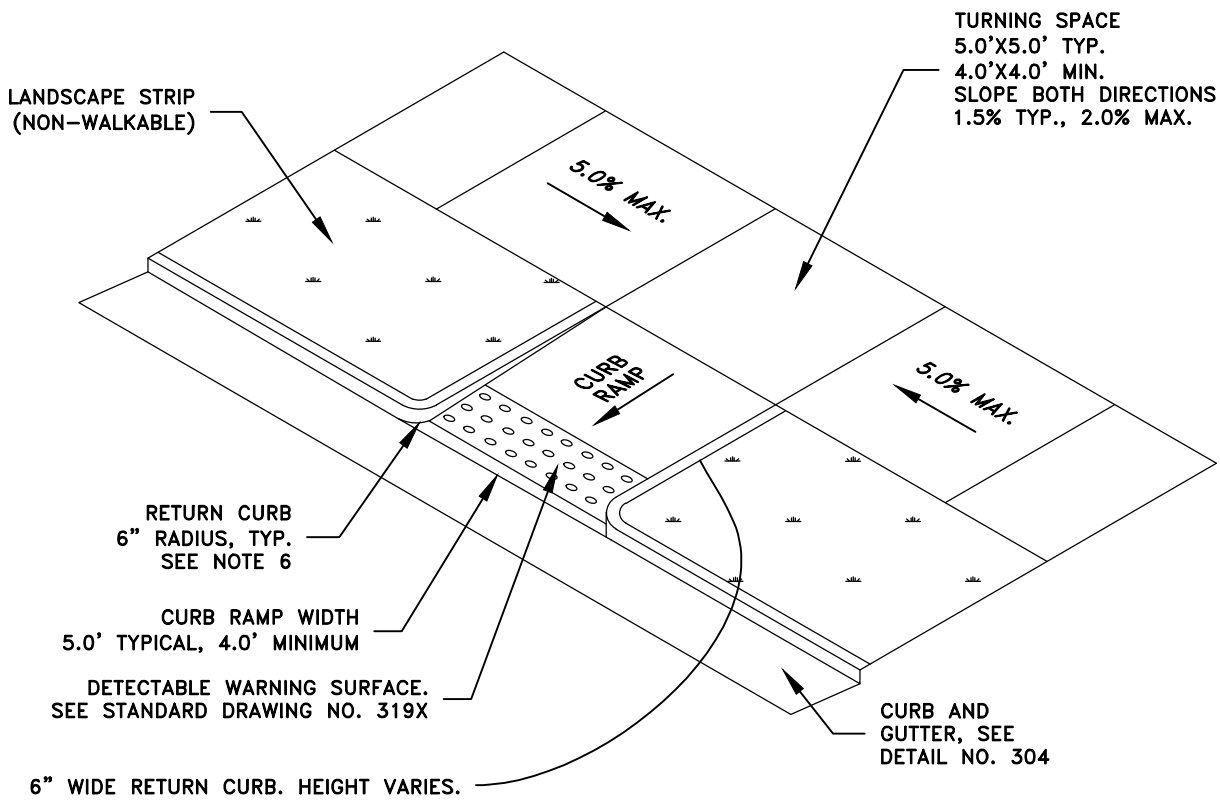
**CURB RAMP CROSS SECTION**



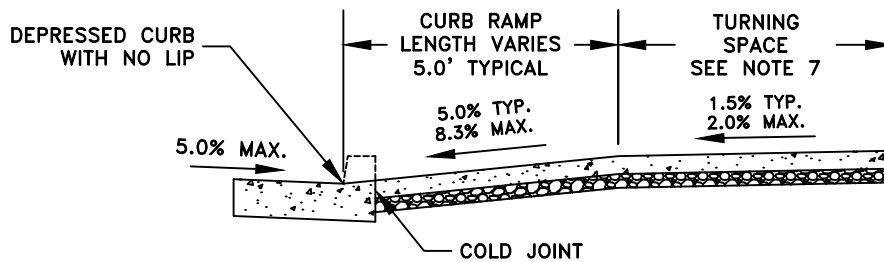
**NOTES:**

1. THE LOCATION AND GEOMETRY OF CURB RAMPS SHALL BE DESIGNED BY THE PROJECT ENGINEER IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT, THE ALBANY DEVELOPMENT CODE, AND THE ALBANY ENGINEERS STANDARDS.
2. ALL CURB RAMPS SHALL COMPLY WITH THE CURRENT ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG) PUBLISHED BY THE US ACCESS BOARD.
3. RAMPS SHALL CONFIRM TO THE REQUIREMENTS OF STANDARD DRAWING NO. 313 FOR PCC AND BASE.
4. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUN SHALL BE 5% MAX.
5. CONSTRUCT FLARED SIDES WITH SLOPE OF 10% MAX, MEASURED PARALLEL TO THE CURB LINE,
6. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE A MINIMUM OF 5.0' IN THE DIRECTION OF THE CURB RAMP.

CITY OF ALBANY, OREGON PUBLIC WORKS DEPARTMENT		
<b>PERPENDICULAR CURB RAMP</b>		
NO SCALE	JANUARY 2023	NO. 316



**CURB RAMP CROSS SECTION**



**NOTES:**

1. ONLY ONE RAMP SHOWN FOR CLARITY. TYPICAL CURB RETURN WILL HAVE TWO RAMPS.
2. THE LOCATION AND GEOMETRY OF CURB RAMPS SHALL BE DESIGNED BY THE PROJECT ENGINEER IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT, THE ALBANY DEVELOPMENT CODE, AND THE ALBANY ENGINEERS STANDARDS.
3. ALL CURB RAMPS SHALL COMPLY WITH THE CURRENT ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG) PUBLISHED BY THE US ACCESS BOARD.
4. RAMPS SHALL CONFIRM TO THE REQUIREMENTS OF STANDARD DRAWING NO. 313 FOR PCC AND BASE.
5. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUN SHALL BE 5% MAX.
6. RETURN CURBS MAY BE PROVIDED IN LIEU OF FLARED SIDES ONLY IF PROTECTED FROM TRAVERSE TRAVEL BY LANDSCAPING, HANDRAIL OR OTHER OBSTRUCTION. RETURN CURB SHALL NOT REDUCE WIDTH OF APPROACHING SIDEWALK.
7. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE A MINIMUM OF 5.0' IN THE DIRECTION OF THE CURB RAMP.

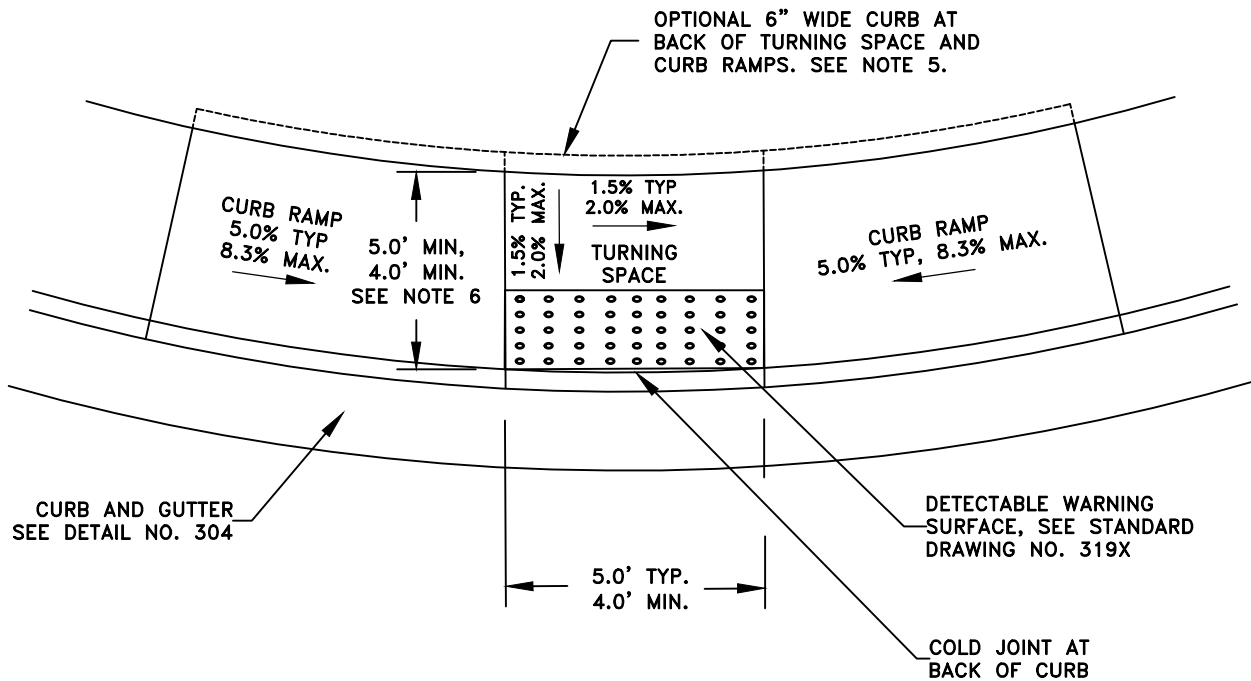
CITY OF ALBANY, OREGON  
PUBLIC WORKS DEPARTMENT

PERPENDICULAR CURB RAMP  
WITH RETURN CURBS

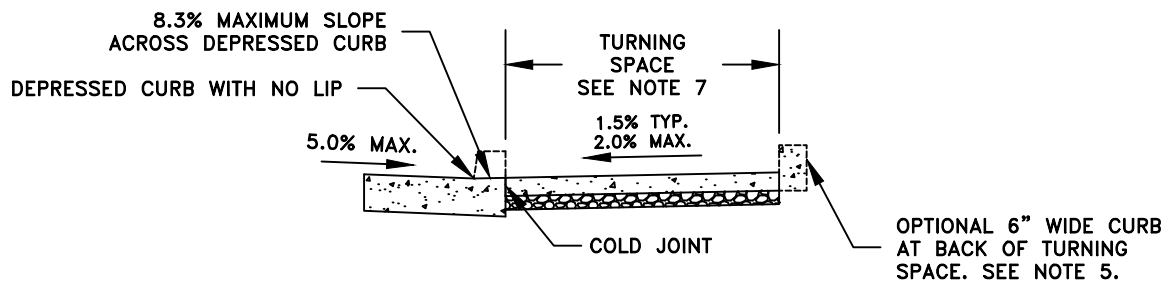
NO SCALE

JANUARY 2023

NO. 317



TURNING SPACE CROSS SECTION



NOTES:

1. THE LOCATION AND GEOMETRY OF CURB RAMPS SHALL BE DESIGNED BY THE PROJECT ENGINEER IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT, THE ALBANY DEVELOPMENT CODE, AND THE ALBANY ENGINEERS STANDARDS.
2. ALL CURB RAMPS SHALL COMPLY WITH THE CURRENT ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG) PUBLISHED BY THE US ACCESS BOARD.
3. RAMPS SHALL CONFIRM TO THE REQUIREMENTS OF STANDARD DRAWING NO. 313 FOR PCC AND BASE.
4. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUN SHALL BE 5% MAX.
5. WHERE THE TURNING SPACE IS CONSTRAINED ON TWO OR MORE SIDES, THE TURNING SPACE SHALL BE A MINIMUM OF 5.0' IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.

CITY OF ALBANY, OREGON  
PUBLIC WORKS DEPARTMENT

PARALLEL CURB RAMP

NO SCALE

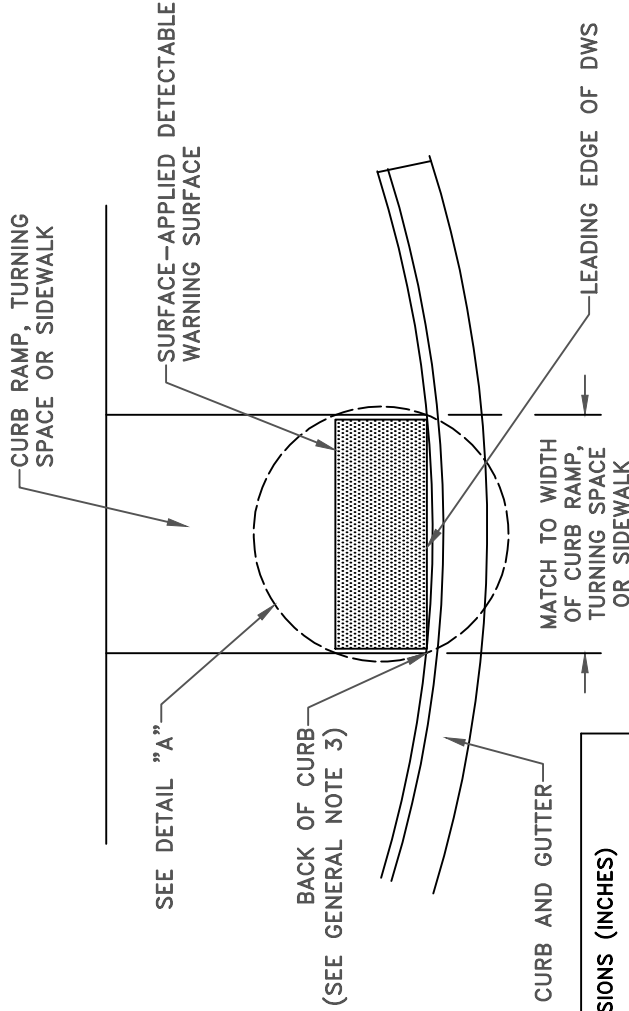
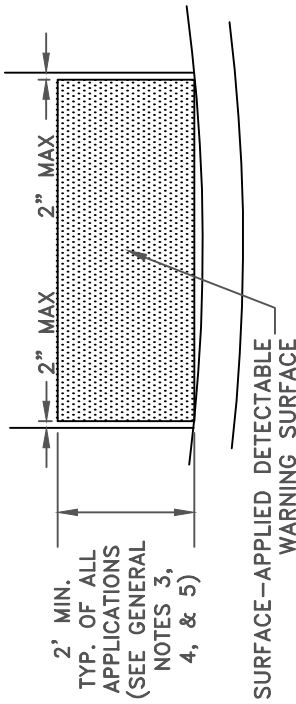
JANUARY 2023

NO. 318

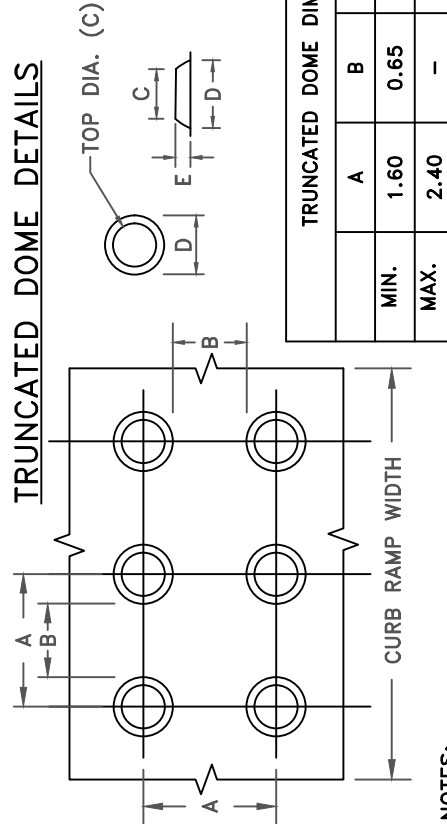


### DETECTABLE WARNING SURFACE DETAIL

#### DETAIL "A"



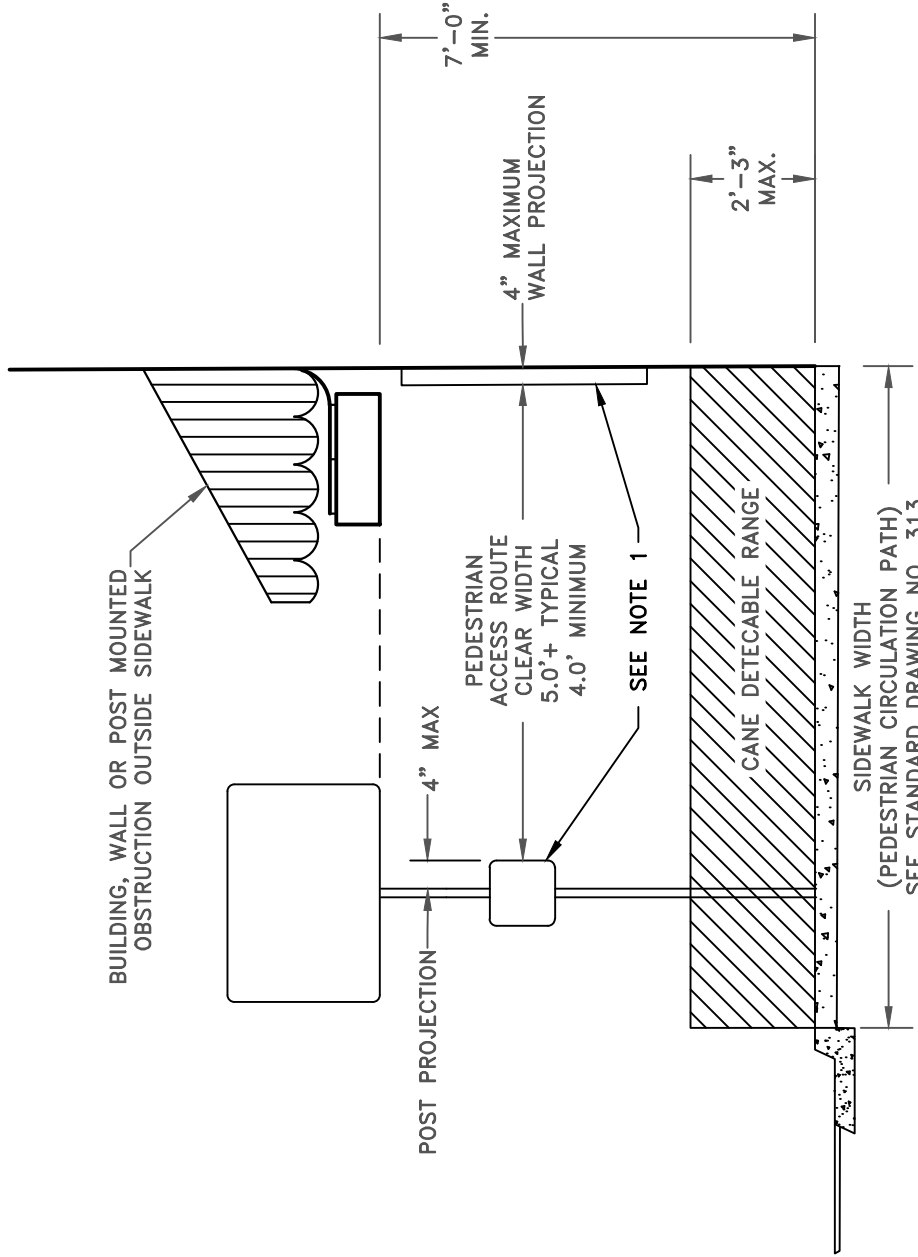
#### TRUNCATED DOME DETAILS



#### NOTES:

1. THE DETECTABLE WARNING SURFACE SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP OPENING, SHARED PATH, BLENDED TRANSITION, TURNING SPACE, OR OTHER ROADWAY ENTRANCE AS APPLICABLE. A GAP OF UP TO 2 INCHES ON EACH SIDE OF THE DETECTABLE WARNING SURFACE IS PERMITTED.
2. DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE BACK OF CURB FOR A MINIMUM DEPTH OF 2 FT. IN THE DIRECTION OF PEDESTRIAN TRAVEL AT CURB RAMPS THAT ARE ADJACENT TO TRAFFIC. DETECTABLE WARNING SURFACE MAY BE RADIAL OR RECTANGULAR, BUT MUST COMPLY WITH THE TRUNCATED DOME SIZE AND SPACING STANDARDS. DETECTABLE WARNING SURFACE MAY BE CUT TO MEET NECESSARY SHAPE AS SHOWN ON THE CONSTRUCTION DRAWINGS. THE DETECTABLE WARNING SURFACE SHALL NOT BE PLACED ACROSS A GRADE BREAK.
3. DETECTABLE WARNING SURFACE SHALL BE USED IN THE FOLLOWING LOCATIONS:
  - A). CURB RAMPS AT STREET CROSSINGS
  - B). PEDESTRIAN ISLANDS (ACCESSIBLE ROUTE ISLANDS)
  - C). RAILROAD CROSSINGS
4. WHERE NO CURB IS PRESENT, THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE EDGE OF PAVEMENT.
5. TRUNCATED DOME COLOR SHALL BE SAFETY YELLOW, EXCEPT IN DOWNTOWN ALBANY WHERE THEY SHALL BE BLACK IN COLOR. ALTERNATIVE COLORS REQUIRE PRIOR APPROVAL OF THE CITY ENGINEER.

CITY OF ALBANY, OREGON PUBLIC WORKS DEPARTMENT	
DETECTABLE WARNING SURFACE DETAIL	
NO SCALE	JANUARY 2023
NO. 319	



CLEAR CIRCULATION PATH

NOTES:

1. OBJECTS WITH BASE BELOW 2'-3" MAY PROTRUDE ANY DISTANCE AS LONG AS THE PEDESTRIAN ACCESS ROUTE CLEAR WIDTH IS MAINTAINED. WHEN AN OBJECT WITH A BASE HIGHER THAN 2'-3" PROTRUDES FURTHER THAN 4" PROVIDE A DETECTION BELOW PROTRUSION TO DELINEATE EDGE.
2. OPENINGS IN THE SIDEWALK SHALL NOT ALLOW THE PASSAGE OF A 1/2" DIAMETER SPHERE

CITY OF ALBANY, OREGON  
PUBLIC WORKS DEPARTMENT

PEDESTRIAN ACCESS  
CIRCULATION PATH

NO SCALE

JANUARY 2023

NO. 320